

Application Number	16/01548/AS
Location	Wye School, Kempe Centre, Olantigh Road, Wye, Ashford, TN25 5EJ
Grid Reference	05666/47023
Parish Council	Wye and Hinxhill
Ward	Wye
Application Description	Phases 2 and 3 of the proposed Wye school expansion comprising permanent use of the Kempe Centre for school use; refurbishment of the Kempe Centre to include new sixth form accommodation and minor alterations to the external appearance of the building to reflect internal reconfiguration; retention of the two existing temporary classroom cabins until the end of the 2018/2019 academic year; demolition of existing structures and some trees; erection of a new building comprising the main hall, 4 court sports hall and new teaching accommodation; new coach, car and cycle parking provision; new soft and hard landscaping; off-site highway works on Olantigh Road and other associated works.
Applicant	United Learning Trust
Agent	Miss A Schembri, RPS Planning and Development Ltd, 140 London Wall, London EC2Y 5DN
Site Area	0.91 ha

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|-----------|-------|---|
| (a) 35/5R | (b) R | (c) KCC (H&T) R
KCC (Heritage) –
KCC (Biodiversity) -
EH (EP) –
PO (Drainage) – |
|-----------|-------|---|

Amended plans:

- | | | |
|-----------|-------|---|
| (a) 35/2R | (b) R | (c) KCC (H&T) –
Kent Downs AONB unit - R |
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Introduction

1. This application is reported to the Planning Committee because it is a major application.

Site and Surroundings

2. The site is located on the north eastern edge of the village of Wye within the Kent Downs Area of Outstanding Natural Beauty. It forms the greater part of the site recently purchased by Wye school (excluding the playing field) together with a section of public highway (Olantigh Road) at the corner of the site with Occupation Road. It comprises the existing main school building (Kempe Centre) on the street frontage with two temporary classroom cabins to the rear. The remainder of the site is mostly hardstanding (playground; outdoor sports courts; access drive and parking areas) with planting confined to boundaries and formal planting within the hard areas.
3. The Kempe Centre and most of the adjoining land/buildings to the west and east of the site form part of the former Wye College complex of buildings. Immediately to the north of the site is an area of woodland, the Arboretum, which has recently been protected by a group Tree Preservation Order and beyond that the ADAS site (also part of the former Wye College). To the north-west of the site, on the opposite side of Olantigh Road are three detached residential units. Residential development also lies to the south of the site on the opposite side of Occupation Road.
4. A site location plan is shown in Figure 1 below.



Figure 1: Site Location Plan

Proposal

5. The proposal is a full application. The application is for the second and third phases of the expansion of Wye School. (The first phase, approved 2016, was for a multi-use games area (MUGA), soft play area and new entrance square.)
6. This application comprises:
 - the permanent use of the Kempe Centre for school use
 - the refurbishment of the Kempe Centre to include new sixth form accommodation
 - minor alterations to the external appearance of the building to reflect internal reconfiguration
 - retention of the two existing temporary classroom cabins until the end of the 2018/2019 academic year;
 - demolition of existing structures and some trees;
 - erection of a new building comprising the main hall, 4 court sports hall and new teaching accommodation;
 - new coach, car and cycle parking provision;
 - new soft and hard landscaping;
 - off-site highway works on Olantigh Road and other associated works.
 - new car and coach park
7. The proposed new building would be located to the north of the Kempe Centre and classroom cabins on what is currently a sports court. It would have a contemporary form and design comprising two flat roofed elements alongside each other: a two storey element (the sports hall) adjacent to Olantigh Road of dimensions 45.18m (length) x 20.03m (depth) x 8.4m (height) and a three storey classroom element addressing the sports field to the east of dimensions 62.97m (length) x 10.57 m (depth) x 11 m (height). A proposed site layout plan is shown in figure 2 below:

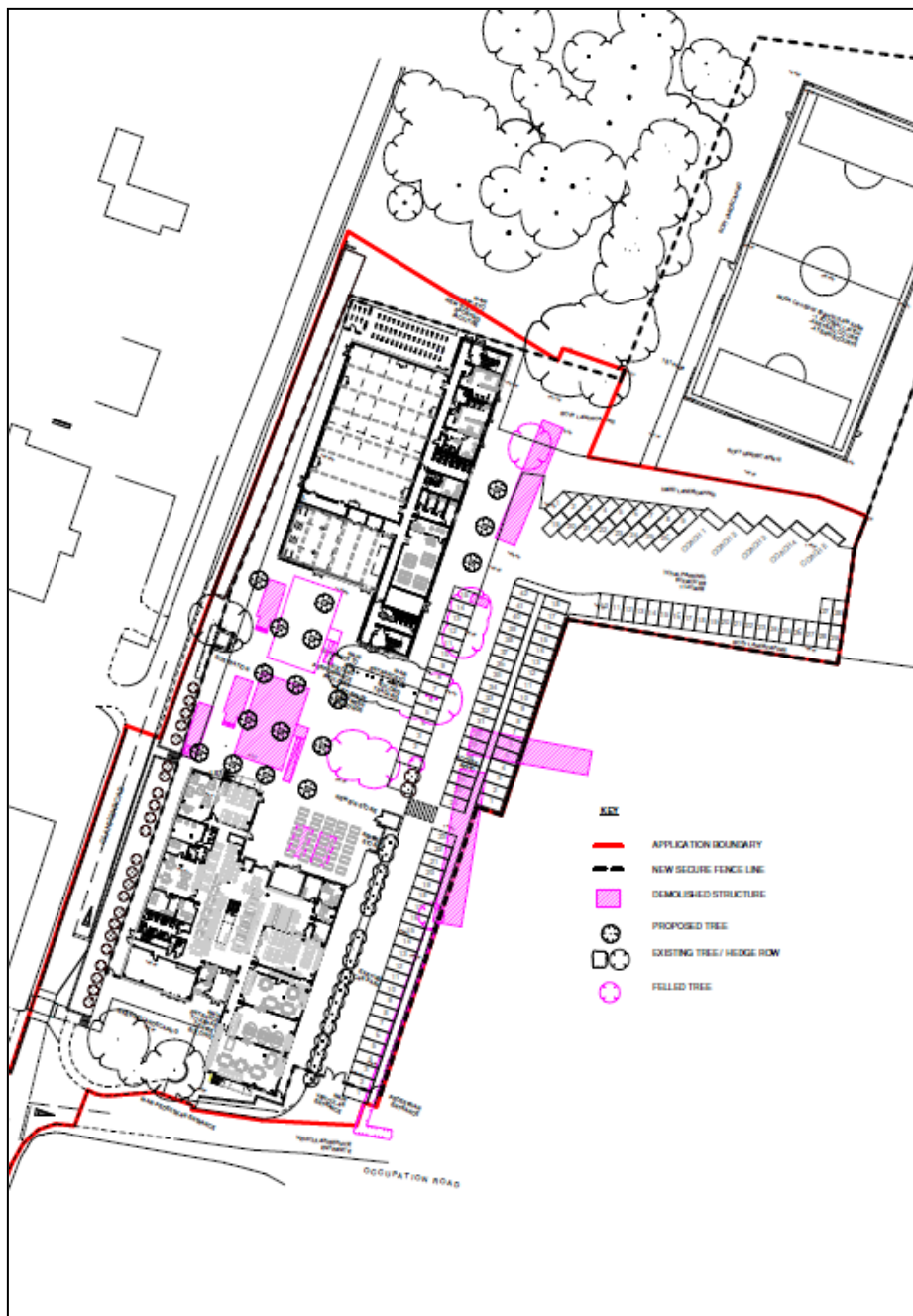


Figure 2: Proposed site layout plan

8. The sports hall element would be of timber cladding, the vertical timber spacing varying across panels. The classroom element would be of two colours of brick with windows arranged in vertical strips with panelling between floors. Selected plans and elevations of the proposed building are shown in Figures 3, 4 and 5 below.

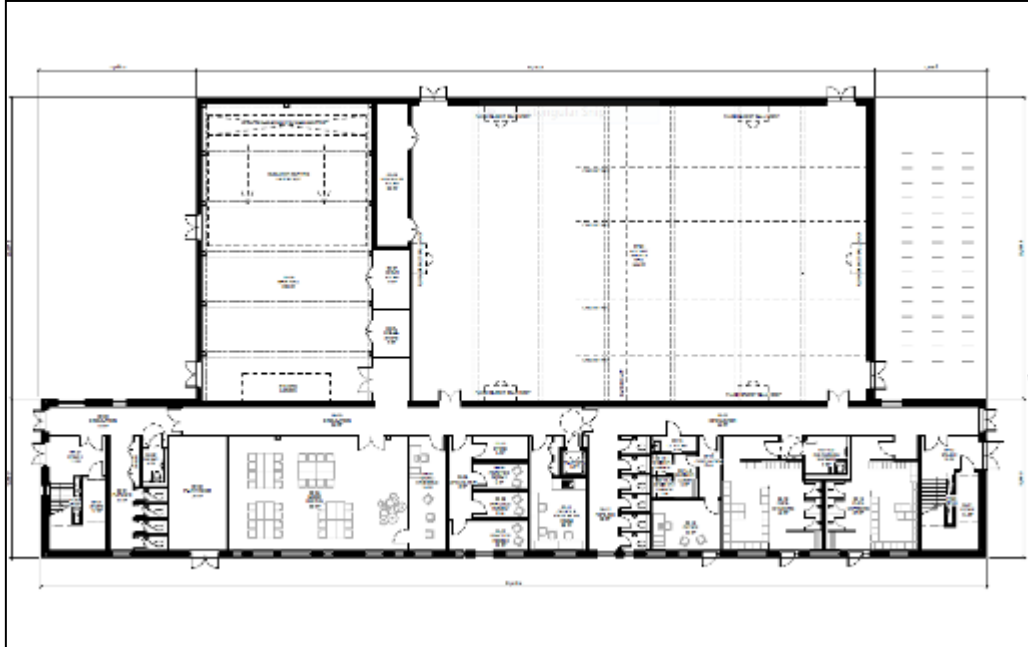


Figure 3: Proposed ground floor plan

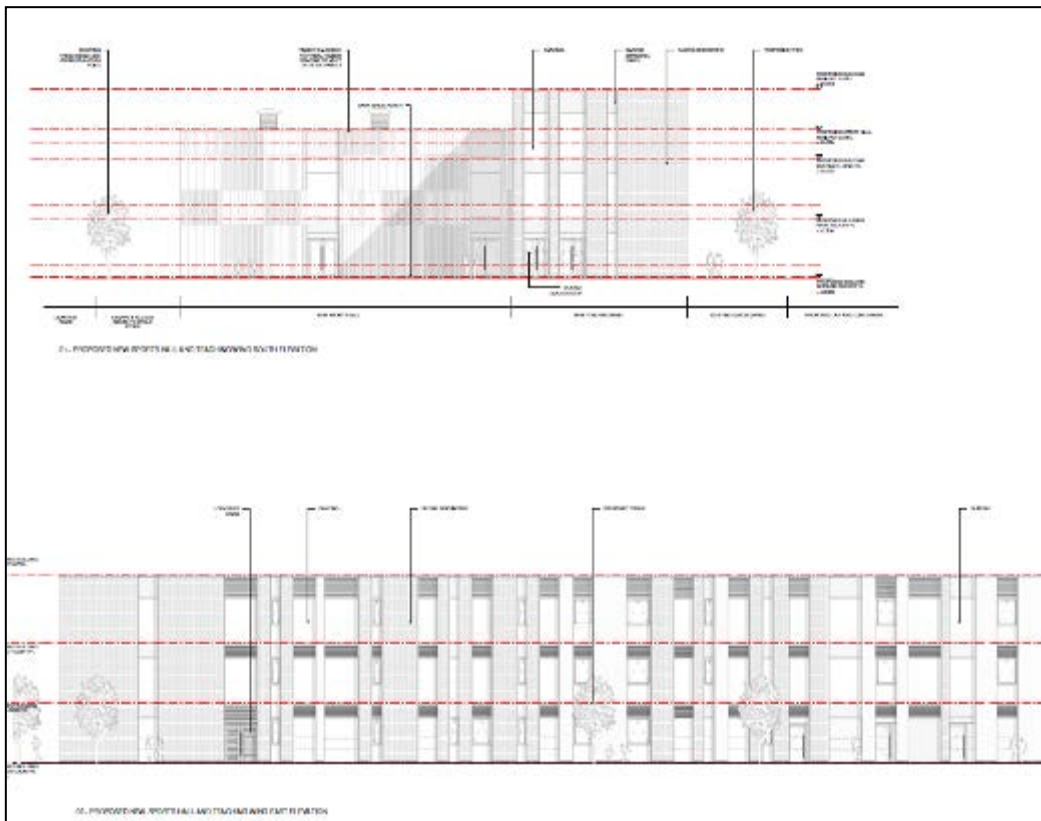


Figure 4: Proposed south (top image) and east elevations



Figure 5: Coloured up images of east (top) and west elevations

The proposed new car and coach park would provide a total of 42 parents spaces for drop off/pick up (available outside of pick up /drop off times); 26 spaces for staff and 15 spaces for sixth formers together with 5 coach parking spaces on a tarmacked area at the eastern end of the site. Many of the car parking spaces would be arranged in tandem due to space constraints. A parking layout plan is shown in figure 6 below:

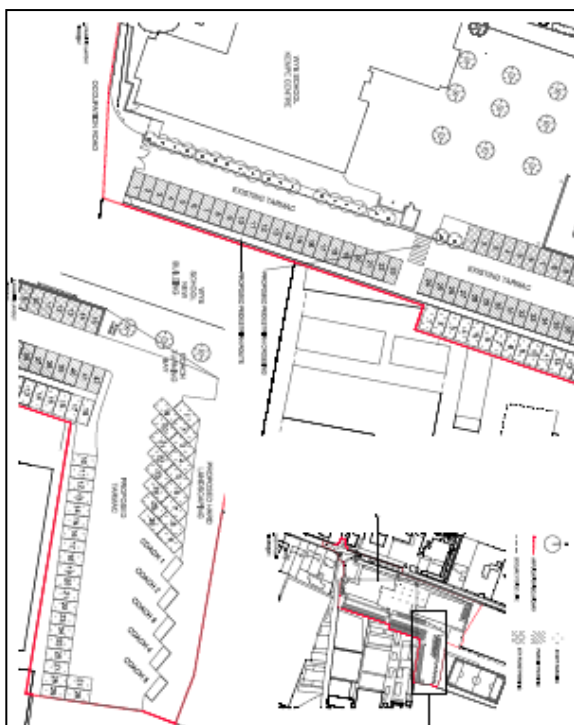


Figure 6: proposed parking layout

9. Since the application was first submitted, the site boundary has been extended to take in a short section of Olantigh Road to the west for the inclusion of traffic calming measures within this street and to provide the necessary site lines. A revised parking and coach parking layout has also been provided to take on board comments made by KCC Highways and Transportation to include additional parking for sixth formers and space on site for drop off and pick up. The position of the proposed building on the site has been shifted by 1m to the east; its footprint has been marginally reduced and some minor changes made to its design. The site boundary has been extended to take in a section of Olantigh Road and the parking area to the east. A Visual Impact Assessment has also been submitted in support of the application.
10. The proposed traffic calming measures are shown in Figure 7 below:

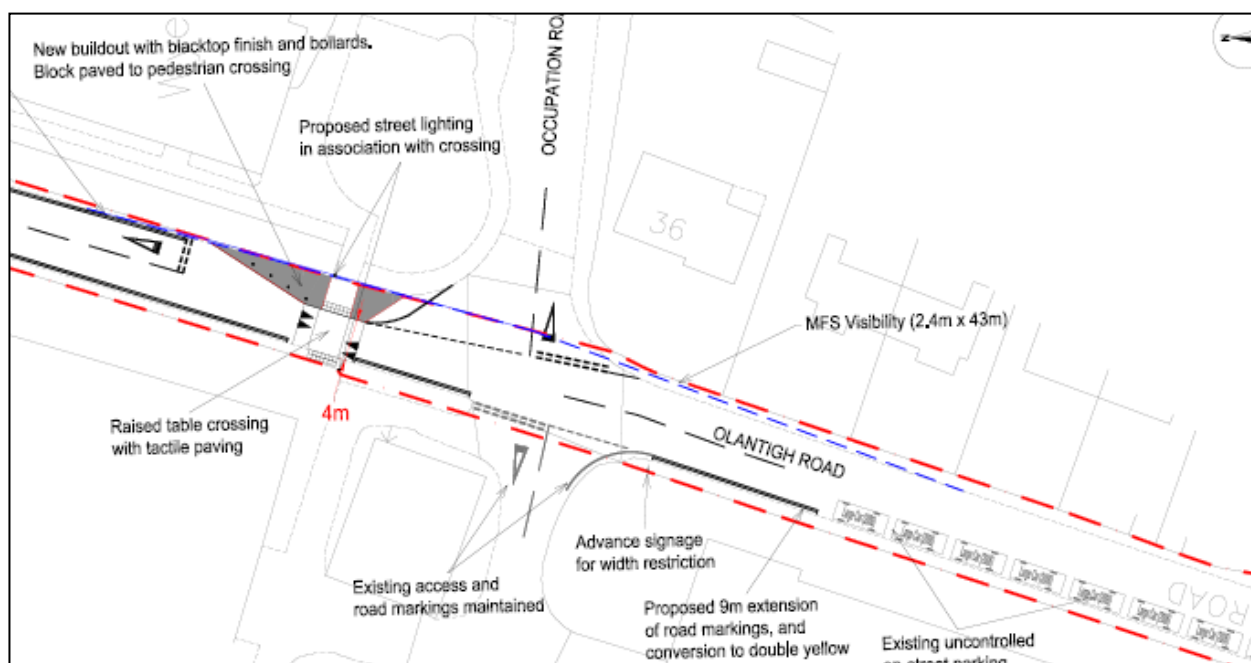


Figure 7: Proposed traffic calming

11. In support of the application the following have been submitted:
12. Design and Access Statement, Architecture Initiative, August 2016 Provides background information setting out the strategy for the school site and showing this development in its context.
13. Planning Statement, RPS, August 2016 considers the extent to which the scheme accords with relevant planning policies stressing the fact that the school's expansion is needed and highlights the community benefits of this proposal.
14. Pre-development Arboricultural Survey, Middlemarch Environmental, June 2016 The site contains a number of early mature and mature trees which are predominantly in a good condition. Also within the study area, are a number of trees and tree groups which are deemed to be notable trees within the landscape with high Category A retention value. These include a pair of Copper Beech; a mixed species group and two areas of woodland along the northern boundary.
15. Arboricultural Impact Assessment, Middlemarch Environmental, July 2016 The proposed development has been designed so that major works do not fall within the RPAs of retained trees. However, the installation of a boundary fence and removal of hard surfacing will require some works within RPAs including some access facilitation pruning works. A total of 11 category 'U' and 'C1' trees, mostly silver birch are identified for removal together with two tree groups, one category 'B', the other 'B3'. The mixed species category 'B'

group is to be part removed back to the boundary due to encroaching vegetation and the B3 group is to be removed to provide construction access into the site.

16. Ecology Report, Greenlink Ecology Ltd, 28th July 2016 Survey work has identified the need for mitigation measures for bats and breeding birds as a precautionary approach. It also identifies various ecological enhancements to improve the biodiversity of the site.

Letter from Greenlink Ecology Ltd, dated 11.10.16 provides details in regards to the reptile mitigation methodology to address concerns raised by KCC Biodiversity. It includes identification of a receptor site.

17. Transport Statement, RPS The results from the school travel surveys demonstrate that the school has a significant number of staff and pupils travelling by sustainable modes of transport at over 89% using bus, rail or walking. Approximately 11% travel by car. The phase 2/3 proposal would deliver an additional 51 cycle parking spaces (there are currently 35 spaces).

The proposed on-site coach parking will facilitate drop off and pick up within the school site, where it currently takes place.

18. Addendum to Transport Statement confirms that there is no change to the overall conclusion of the original statement that the development proposals provide a safe means of access to the site and would not have a severe residual impact on the operation of the local highway or transport networks. Indeed, the changes represent a betterment of the original proposals.
19. Travel Plan 2016/17 aims to instil positive habits whilst the school is expanding by promoting sustainable transport. It has a longer school day (opens 7:30 with after school activities until 17:30) to allow parents the chance to work normal hours. A dedicated bus service is provided by Poynter's Coaches for students travelling from the Ashford direction. There will be four school buses for the 2016/17 academic year, each carrying around 60 students. Stagecoach runs a service from the Canterbury direction which is used by some students. Regular travel surveys are carried out and reviews of the travel plan. Parents and students are provided with a Travel Information Pack and car sharing is encouraged.
20. Historic Environment Desk Based Assessment, Archaeology South East identifies a moderate/high potential impact for archaeological remains to be present acknowledging that past development is likely to have had a significant localised impact. It recommends further archaeological investigation and trial trenching.

21. Geo-environmental Assessment recommends further gas monitoring at the site.
22. BREEAM Pre-Assessment
23. Services and Environmental Strategy and Renewable Energy Statement concludes that the most practical and cost effective solution for achieving a 10% reduction in Carbon is the implementation of PVs at roof level.
24. Wye School Visual Impact Assessment (March 2017) which shows there are only a few instances from where the newly proposed hall and teaching block will be seen from listed buildings and that these views are not harmful due mainly to the distances involved and intervening built form.

Planning History

15/00754/AS Temporary permission for the siting of 2 no. two storey and 2 no. single storey modular buildings in association with school and additional hardstanding until the end of the 2017/18 academic year to allow for the Wye Free School's expansion to 450 pupils and 43.5 staff. (The decision allowed temporary use of the Kempe Centre as a school until 31st July 2017). Approved June 2015

15/00598/AS - Variation of Condition 5 on application 13/00521/AS to extend the use of the building as a Free School until 31st July 2017. Approved April 2015

13/00521/AS - Removal of Condition 4 on planning permission 94/0472/AS to allow use of the Learning Resource Centre as a Free School. Approved July 2013

94/0472/AS New two storey learning resource centre and parking for 155 cars. Approved June 1994

16/01226/AS Demolition of existing outbuildings and removal of trees; resurfacing of the existing vehicular access road off Occupation Road; creation of a new Multi-Purpose Games Area (MUGA) and soft play (grassed) area; creation of a new entrance square plus site levelling, new lighting fixtures and associated works

Consultations

First Consultation

Ward Member: The Ward Member is a member of the planning committee. No formal comments have been received.

Historic England: recommends that the application should be determined in accordance with national and local policy guidance and on the basis of specialist conservation advice

KCC Highways and Transportation: recommends refusal on the following highway grounds:

- Insufficient off site highway works to mitigate traffic and safety impacts of the development
- Insufficient on site car parking (for sixth formers who may choose to drive to school)

It also highlights the need for improved footway provision on the western side of Olantigh Road.

KCC Heritage: No objection subject to conditions

KCC Biodiversity: No objections subject to conditions

KCC Public Right of Way Unit: raises no objections noting that whilst a bridleway passes adjacent to the proposed site (AE109) the expansion works are unlikely to have an impact

Environmental Protection: No objection subject to a condition regarding the need for gas monitoring

Project Office Drainage: No objection subject to a condition

Wye and Hinxhill Parish Council: objects on the grounds that the application is premature. It urges the Borough Council to consider this application as part of the Wye 3 master planning process.

Neighbours: 35/5R

- The original proposal to convert the Edwardian buildings into a school bringing a vacant building back into use had merit; this does not and the Edwardian buildings are falling into a state of disrepair
- The wrong site for this use
- The site is too small to accommodate the scale of development proposed
- The height and scale of the building is inappropriate in this location – it looks like the sort of building you would find in an urban setting. If it were an office block or warehouse would it be allowed?
- There are no commercial buildings of this height in Wye and the construction will effectively appear higher than a three storey building setting a precedent for

development across the rest of the WYE3 area. The parapet height is in excess of 1.5m above the eaves height of the Kempe Centre

- The site is on raised land – 1.8m above street level - so it will appear even higher
- The new building would be too close to Olantigh Road impacting upon the entrance into the village and the private dwellings opposite
- Negative impact on AONB
- There are a number of empty buildings in disrepair on the other side of the street which the school could occupy
- The plans for the school have gone way beyond the original proposal for a temporary school on the site
- Increased traffic as a result of the increase in pupil numbers, including sixth formers who may choose to drive to school. Existing traffic is already causing environmental damage
- Inadequate parking provision on site
- Difficulties of coaches accessing the site – there are existing problems which have been highlighted by a resident – Why not introduce walking buses so that coaches do not need to enter the site?
- Disruption to neighbours during construction
- The proposed development would bring to fruition a project that has been supported by both ABC and Wye PC and which is supported by local surveys
- The Wye School is oversubscribed and would benefit from additional buildings
- There is support for the school in the Neighbourhood Plan
- The proposed development would provide an attractive modern building - information submitted with the application demonstrates that it would not be dominant in views. It is suggested however that the building is turned through 45 degrees to reduce its impact
- The adaptation of the Kempe Centre for school use has happened without it losing its distinctive features
- It should only be approved on the basis that the Russell Laboratories are replaced with open space

- Consideration should be given to the impact of westward views

Consultation on amended plans – additional comments

KCC Highways and Transportation: raises no objections subject to conditions stating that the applicant has now produced a revised parking layout which provides adequate numbers for the school staff, sixth form pupils and an area for parents to pick up and drop off pupils.

Tracking diagrams have been produced to show that the school coach access arrangements will work and an element of tandem parking for staff has been permitted on the basis that this is something the staff can manage amongst themselves.

Kent Downs AONB Unit: highlights the high level of protection accorded to AONBs, pointing out the need to assess the proposal against Policies SD2, SD8 and SD9 of the Kent Downs AONB Management Plan, which is a material consideration.

The AONB Unit has concerns that the scale of the proposed new building is inappropriate in this edge of village, rural setting. It is concerned particularly about the height and scale of the three storey teaching accommodation and would welcome consideration of ways of reducing the height and bulk of this, particularly in view of the modular nature of the accommodation which could be broken up, unlike the required sports space. While it is noted that the existing Kempe building incorporates a three storey element, three stories are considered out of context in the village where development is generally restricted to two stories. The height would be exacerbated due to the rising ground on which the site lies, and while it is noted that the three storey element is set back from the road behind the proposed sports hall, the scale would make the building more prominent in views from the east where the site is visible from a number of PROW and permissive paths within the wider countryside, including the North Downs Way.

The proposed use of vertical timber cladding on the sports hall is considered an appropriate contemporary approach that will help assimilate the building into its rural surroundings and help break up the mass of the building. The proposed use of a dark red multi stock is also considered appropriate and the use of a light coloured mortar to complement this is advocated. The extensive glazing on the eastern elevation is of concern; in order to reduce impact in views from the east it is imperative that non-reflective glazing is used on this elevation. Concern is also expressed that rooms will often be lit increasing the visibility of the building when viewed from the east. While the proposed use of aluminum curtain walling and louvres are not considered to aid local distinctiveness of this historic rural Kent Downs settlement, the proposed colour pallet of dark muted colours is considered appropriate.

Additional planting should be incorporated to help assimilate the building into its rural surroundings. In particular, there is considered to be scope for additional trees to be planted along the eastern boundary of the building and that trees which have the capacity to establish large crowns should be utilised to assist in filtering views of the development from the public rights of way to the east. In addition, it is noted that the proposed planting treatment for Olantigh Road is limited to grass. The frontage along Olantigh Road is generally hedged as you travel in a northerly direction from the school and it is considered important that this is maintained. The Unit would like to see either a mixed native species hedge planted along the road boundary, or if a more formal approach is required, a beech hedge. It would also like to see tree planting incorporated into the new car park area to assist in breaking up the expansive area of hardstanding, given the location of the site on the rural edge of the village along with the incorporation of structural landscaping along the eastern boundary of application site which would help filter views of the proposed car park and building.

It is essential to ensure that the character of the rural lane (Olantigh Road) is not eroded by the proposed alterations to the highway. The proposed treatment of the road build out will be crucial and the proposed black top finish with bollards is considered an inappropriate urban standardized solution. It would welcome the opportunity to consider a more sensitive solution appropriate to this rural location, along with a limitation on white lining and other road markings. Consideration should be given to using flint, setts or bound gravel for the build out and bollards, if a necessity, should be timber. The number of signs should be kept to a minimum and amalgamated and clustered with other street furniture. Appropriate surface treatment of the raised table is also required and we would support a restrained use of coloured surfacing (buff is generally more appropriate than red).

Environmental Protection: No further comment regarding amended plans

Wye and Hinxhill Parish Council: objects on the following grounds:

- The application is both piecemeal and premature in policy terms and should not be considered in advance of the WYE3 masterplan
- The proposal is contrary to Policy WNP11j of the adopted Wye Neighbourhood Plan which requires WYE3 to be considered as a whole and considers the overall redevelopment of the campus landholding
- As a major site in the heart of WYE3 it would result in profound and unresolved highway safety, road junction and traffic issues
- It would result in negative visual impacts from both the perspective of immediate neighbours and the AONB

- The combined impacts of the development would affect the function of Wye as a village and its appearance from a large area of the surrounding protected countryside
- Objects to the development of greenfield land for parking and school bus use when there is plenty of brownfield land in the village
- Concerns about highway safety, including a lack of speed calming measures and inadequate on-site parking provision. Considers that the amended design compounds safety issues as bus drivers will need to reverse into areas with desire lines. This will be compounded by inexperienced sixth form drivers within the area. The on-site access arrangements point to an overdevelopment of the site
- The proposal is contrary to Policies in the Kent Downs AONB Management Plan which seek to improve tranquillity, including dark skies at night
- The proposal is an overdevelopment of the height with a scale, massing and height inappropriate in this location. The scale is monolithic giving a vertical east elevation of some 750 sq m.
- Why has the enlargement of the site not triggered a new application?
- The design is industrial using materials that are incongruous in the village eg. extensive areas of polyester powder coated aluminium panels and louvres. The spandrel glass is a particularly jarring element due to its highly reflective qualities which would draw attention to the building
- The application does not state how it accords with the Wye Village Design Statement
- There is a lack of capacity in the sewerage system – queries about whether the proposal meets building regulations
- The flat roof plan makes no mention of roofing materials or colours but mentions a 71 sq.m. PV array and MEP plant. These elements are likely to have a negative visual impact on the landscape.
- The proposal to dispose of all surface water from the roofs and into a soakaway is wasteful – rainwater harvesting is required
- There appears to be no provision for handling inevitable oil and fuel spills in the parking area

Neighbours: 35/2R

- The mitigation is not sufficient to address previous concerns
- All the changes are confusing
- The best site for this use is the ADAS site as identified at the workshop session

Planning Policy

25. The Development Plan relevant to this application comprises the saved policies in the adopted Ashford Borough Local Plan 2000, the adopted LDF Core Strategy 2008, the adopted Ashford Town Centre Action Area Plan 2010, the Tenterden & Rural Sites DPD 2010, the Urban Sites and Infrastructure DPD 2012, the Chilmington Green AAP 2013 and the Wye Neighbourhood Plan 2015-30. On 9 June 2016 the Council approved a consultation version of the Local Plan to 2030. Consultation commenced on 15 June 2016 and ended on 10th August 2016. At present the policies in this emerging plan can be accorded little or no weight.
26. The relevant policies from the Development Plan relating to this application are as follows:-

Ashford Borough Local Plan 2000

EN10 - Development on the edge of existing settlements

Local Development Framework Core Strategy 2008

CS1 - Guiding Principles

CS2 - The Borough Wide Strategy

CS9 – Design Quality

CS15 – Transport

CS18 – Meeting the Communities Needs

CS20 – Sustainable Drainage

Tenterden & Rural Sites DPD 2010

WYE3 – Imperial College, Wye

TRS17 – Landscape Character and Design

TRS18 – Important Rural Features

Draft Local Plan to 2030

SP1 – Strategic Objectives

SP6 – Promoting High Quality Design

Wye Neighbourhood Plan 2015-30

WNP6 – Mixed development

WNP11 - The former Imperial College London campus at Wye

27. There are two Wye Neighbourhood Plan (WNP) policies of particular relevance to this site, policies WNP6 and WNP11. These are written in full below:

Policy WNP6 *Mixed development*

Development proposals for the WYE3 site should deliver a mix of uses, including education, business, community infrastructure and some housing. Given the scale of the site in relation to the village, such development should be delivered in a phased manner in accordance with a masterplan that has been adopted as a Supplementary Planning Document by Ashford Borough Council.

Prior to any planning application pursuant to the agreed masterplan an application for a screening determination regarding the need for an Environmental Impact Assessment shall be made to Ashford Borough Council. Subject to that opinion any application should be accompanied with an appropriate Environmental Impact Assessment.

Policy WNP11 *The former Imperial College London campus at Wye*

As outlined in Core Policy WNP6, the former Imperial College London landholding at Wye (WYE3) is proposed for a mix of uses, including education, business, community infrastructure and housing. In this regard development proposals for this site shall, subject to viability:

- a) Provide for the continued use of part of the site for education through the establishment of a secondary school or equivalent activity on the site.

Any such development should include the adoption of a travel plan to limit the use of cars to bring staff and students to and from the site prior to the commencement of any such development. Detailed design should be

submitted to and agreed by the local planning authority and the highways authority for the entrance to the school and the Occupation Rd/Olantigh Rd junction in agreement with the developers of the remaining Occupation Rd site prior to the occupation of the buildings for this use.

b) Redevelop part of the site as a business hub (B1 Office or A2 Research and development).

c) Retain and enhance the existing commercial land use along the southern side of Occupation Road for employment use (B1).

Renovation of the properties here would improve the appearance of the entry route into the village from the North Downs Way.

For (b) and (c), adequate parking provision for such commercial floorspace should be provided as part of any such redevelopment.

d) Incorporate the continued use of the land south of Occupation Rd for horticultural businesses.

e) Achieve the positive re-use of the Grade 1 and Grade 2 listed, and other unused Edwardian buildings of the former Wye College by a mix of community, residential and business uses.

Such development would require provision of a new pedestrian and vehicular access route into the site.

Small scale B1 use, live/work units and some residential change of use would also be supported here to encourage the development of a thriving community hub in these historic buildings in the heart of the village.

f) Retain the Withersdane site for institutional, residential (C2) use, subject to traffic generation being compatible with the highway constraints of Scotton Street and the upgrading of footpath links between the site and the village to ensure mobility scooter access.

g) Re-develop areas of land not used by the school or business hub for approximately 50 dwellings.

h) Achieve appropriate reuse of the site of the former ADAS buildings, having regard to the concept of the walkable village.

i) Pay particular attention to the potential for innovation in materials and form in the design of new buildings, and should where possible embody elements such as green roofs and non-reflective materials to limit the visual impact of additional development here on the surrounding AONB. Any development

should provide a connection to the nearest point of adequate capacity in the sewerage network, as advised by Southern Water.

j) Ensure that all additional landscaping provided across the site is of high quality given its setting within the AONB, is comprised of species native to this area of the Kent Downs and of a design, scale and format appropriate to its setting close to the SAC. Applications should demonstrate how proposed landscaping has been designed to enhance views from the AONB.

k) Where appropriate, having regard to the statutory requirements, development of WYE3 will be subject to Section 106 agreements to support traffic calming on Olantigh Road, Scotton Street and Oxenturn Road, improvement to the village hall complex and the establishment of a day care centre for the elderly (see Appendix B).

28. The following are also material to the determination of this application:-

Supplementary Planning Guidance/Documents

Landscape Character Assessment SPD 2011

Sustainable Drainage SPD 2010

Dark Skies SPD 2014

Government Advice

National Planning Policy Framework (NPPF) 2012

29. Members should note that the determination must be made in accordance with the Development Plan unless material considerations indicate otherwise. A significant material consideration is the National Planning Policy Framework (NPPF). The NPPF says that less weight should be given to the policies above if they are in conflict with the NPPF. The following sections of the NPPF are relevant to this application:-

- Para 11 sets out a presumption in favour of sustainable development requiring planning applications to be determined in accordance with the development plan unless material considerations indicate otherwise.
- Para 17 on Core Planning Principles includes the encouragement of the effective use of land by reusing land that has been previously developed (brownfield land), provided it is not of high environmental value.
- Para 17 on Core Planning Principles includes seeking to secure high quality design

- Section 4 promotes sustainable transport and requires all developments that generate significant amounts of movement to be supported by a Transport Statement.
- Para 72 sets out how the Government attaches great importance to ensuring sufficient choice of school places to meet the needs of existing and new communities. It requires local authorities to give great weight to the need to create, expand or alter schools.

National Planning Policy Guidance (NPPG)

Assessment

30. The main issues for consideration are:

- Principle of development
- Wye Neighbourhood Plan
- Landscape impact; Impact on Conservation Area; Visual Amenity
- Highway Safety
- Residential Amenity
- Trees
- Ecology
- SUDS

Background

31. Wye School currently benefits from a temporary planning permission only to operate from the former Kempe Centre but this will expire in July 2017 and permits a maximum school capacity of 450 pupils and 43.5 staff. The school currently has 272 pupils on the school roll in years 7 to 9. This proposal to expand the school seeks consent for the permanent location of the school in its current location in addition to a new sports hall and teaching block. This would allow the school to grow year on year by 90 pupils until it reaches a 600 pupil capacity in 2019/2020. By this time a total of 52 staff would be required.
32. A planning application for the Phase 1 school expansion, which was for a multi-purpose games area (MUGA), was approved under delegated powers earlier this year (Ref 16/01226/AS). This phase 2 and 3 expansion is for the new building required to support the increase in school numbers. This would

generate additional parking and vehicle movements and the current application includes new parking areas and traffic calming / sight lines in Olantigh Road.

Principle of Development

33. The site falls within WYE3 of the adopted Tenterden and Rural Sites DPD which requires that the long term future of the buildings, facilities and land at the former Imperial College campus shall be the subject of an active and comprehensive marketing campaign for educational and related research and business uses for a minimum continuous period of 6 months with the aim of securing a future for such uses on all or part of the campus. This exercise was carried out by Imperial and it is accepted that much of the site/buildings will not remain in an educational/R&D use. However, the use of the Kempe Centre as a free school is policy compliant in this respect. I therefore consider that it is an acceptable use for the WYE3 site. Further support for this approach is provided by the NPPF which requires local planning authorities to give great weight to the need to create, expand or alter schools (para 72).
34. WYE3 states further that if the marketing campaign demonstrates that such uses are not possible on all or part of the WYE3 area, then a master planning exercise encompassing all of the WYE3 area shall be undertaken in order to establish the nature, scale, location and mix of any alternative uses to deliver a high quality, mixed use development. This exercise should inform “a formal review of this policy or the adoption of an SPD based on the agreed masterplan.” A master planning exercise has commenced as part of the neighbourhood planning process with a community and stakeholder workshop taking place in January 2017. Whilst there is currently no formal masterplan for the site, the Workshop established that the use of the Kempe Centre and surrounding site/land as a Free School was widely supported. Support for proposals that meet the community’s needs is found in Core Strategy policy CS18.
35. The recently adopted Wye Neighbourhood Plan (see below) includes policies which are considered to constitute a formal review of Policy WYE3. It identifies the need for a masterplan for the WYE3 site and a further Workshop is planned in June this year.

Wye Neighbourhood Plan

36. Wye and Hinxhill Parish Council has objected to this planning application on a number of grounds including that the proposal is piecemeal and premature in policy terms. They have urged the Borough Council to consider this application holistically and within the context of the ongoing Master-planning process.

37. Whilst it is accepted that this proposal precedes the masterplan, it is not considered to prejudice the masterplan. Policy WNP11 (a) provides for the continued use of part of the former college site for education uses stating amongst other things that a “detailed design should be submitted for the entrance to the school ...”. Furthermore, policy WYE3 is helpful in that it provides guidance should development come forward in the absence of a masterplan. Specifically, it seeks the retention of the main educational uses and provides guidance to any development on land either side of Occupation Road subject to :
- a) The scale, design and use being compatible with the character of the area and not being visually intrusive within the AONB; and
 - b) The development not generating an unacceptable level, or type of traffic:
 - c) The development not generating an unacceptable level, or type of noise or disturbance, or loss of residential amenity.

In light of this, given that continued use of part of the site for education is acceptable in principle in accordance with the development plan, and the proposed application would be conducive with this, provided the proposals represent an appropriate and acceptable form of development when assessed against relevant policies set out in the development plan and guidance within the NPPF the principle of development is considered to be acceptable. This is considered in detail below.

Visual Amenity, Landscape Impact and Impact on Conservation Area

38. The school site is not a large one which has led to the need for a compact building design and three storey classroom element. It has also led to a need for tandem parking within the parking area. A number of objectors including the parish council have pointed to an overdevelopment of the site. Whilst a larger site may have resulted in a different form and massing and more generous parking area, the role for officers is to assess the acceptability of the design and layout in front of them. Sometimes, a tight site can actually lead to a better design. In this case, I believe the compactness of the building form on this site conveys some advantages. Also, the tandem arrangement of parking, whilst inconvenient for users (see section on Highway Safety), does reduce significantly the area of hardstanding that would otherwise be required.
39. This is by its nature a large building. Furthermore, as the Parish Council and a number of objectors have pointed out it would be situated at a higher level to the street. A site section is included in Figure 8 below to demonstrate this point. A perspective is shown in Figure 9 to show the view from Olantigh Road

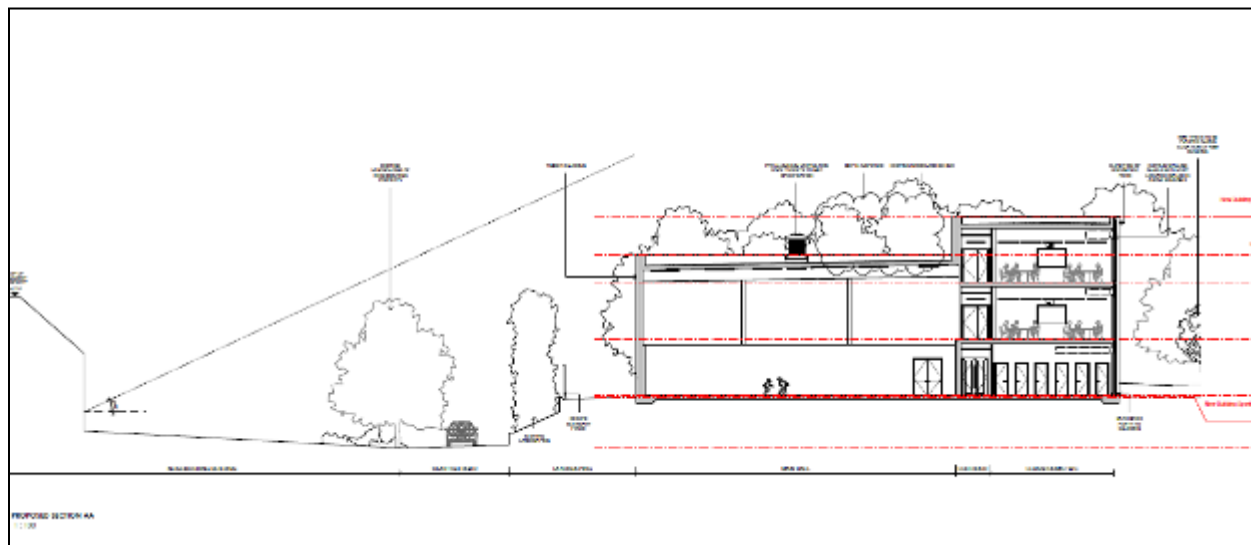


Figure 8: Section across Olantigh Road



Figure 9: Perspective view form Olantigh Road

40. Although the building would be at a higher level to the street by 3.29m it would be set back from Olantigh Road behind a high hedge (about 7m high). It would also be 'contained' to the north by the large trees of the Arboretum and by the existing Kempe Centre to the south. This would help reduce its impact in the street scene. The upper part of the building would be visible from the properties opposite on the other side of Olantigh Road but these are some distance away as shown in the section drawing.
41. The building would be seen in long views from higher land to the east. However, it would not be seen in isolation, but in the context of the existing Kempe Centre which has a not dissimilar scale and the buildings to the west

of Olantigh Road, which are also of some size. Intervening bands of hedgerow/trees would help reduce its impact in long range views from the east. The compactness of the building would in my view assist in this longer range views by limiting the extent of the building footprint across the site. As suggested by Kent Downs AONB Unit, a condition requiring non-reflective glazing in the east elevation on the teaching block is proposed.

42. The applicant's visual impact assessment shows there are only a few instances from where the newly proposed hall and teaching block would be seen from listed buildings and that these views are not harmful due mainly to the distances involved and intervening built form. The conservation area is situated some 245 m to the south of the proposed new building and it would be difficult to make a case that the building would have an adverse impact on the conservation area or its setting. The proposed building has considerable mass and bulk and part of it will be one storey higher than the surrounding buildings. However, from a conservation point of view, I am satisfied that its grouping with similar buildings and its distance from the conservation area will be such that it will not have a significant impact on the setting of the conservation area. I consider that it complies with this part of the NPPF relating to the setting of designated heritage assets.
43. The design of the proposed new building has been amended since first submitted to help 'lift' its design. In particular, a secondary brick colour has been introduced to the north and east elevations to help break up the external appearance of those elevations and the spacing/pattern of the vertical timbers



to the hall have been varied. Figure 10 below shows some building details:



Figure 10: Building details

44. The proposed changes to the external appearance of the Kempe Centre are minor in nature. These include new louvres to some (mostly high level) windows and new flues. I do not consider that they would be harmful to the visual amenity of the area.
45. Whilst I accept this is a large building, I do not consider that the proposed development would be visually intrusive within the AONB; impact adversely on the conservation area or its setting, cause harm to the setting of Listed Buildings or otherwise be harmful to the visual amenity of the area.

Highway Safety

46. This application will result in an increase in pupil numbers and associated trips generating a need for additional on-site parking. The need for on-site parking for sixth formers, who may choose to drive to school, has been highlighted during the consultation process and 15 spaces are not proposed on site. Furthermore, the need to calm traffic in the vicinity of the school at the junction between Olantigh Road and Occupation Road and provide the necessary visibility splays is required on highway safety grounds.

47. KCC Highways and Transportation initially raised objections to the proposal, but following discussions amended plans have been submitted which show an internal layout and off-site highway works on Olantigh Road which are acceptable to them.
48. The proposed parking layout would be compact in form and require management to make it work. Pick up and drop off would be achieved on site with 42 spaces provided for this purpose. These spaces would be available outside of pick up and drop off. KCC Highways and Transportation has commented that with all parking elements as well as the coaches on the same site it may appear a slightly chaotic environment. However, given that the 6th form and parent spaces are likely to leave site slightly before the school coaches, and all of these before the school staff, it is content that the layout should operate safely. Added to which there will be safe pedestrian routes to the parent pick up/drop off point and to board the coaches as well as standard school practice being for staff to be present within the car park as wardens to assist.
49. The proposed highway improvement scheme for Olantigh Road proposes a road narrowing with one-way priority working for traffic accompanied by a small raised table to both slow traffic speeds and improve the pedestrian crossing element. The one-way priority working for traffic will slow vehicle speeds on the approach to the Occupation Road junction as well as to the village beyond and represents a good starting point for any further revisions for Olantigh Road that may accompany future development. This proposed scheme allows for the offset of the give way line for Occupation Road thus improving visibility as well as reducing traffic speeds. It also includes the provision of two sections of double lining on the western side of Olantigh Road to both ensure the road is kept clear for safe operation of the road narrowing, and to allow coaches to turn left out of the access. As such this layout is considered acceptable.
50. Lastly, KCC Highways and Transportation has commented that the revised increase in the parking numbers on site as well as the proposed highway alterations are unlikely to have a material impact on the highway capacity and operation of the Occupation Road junction as the background flows on Olantigh Road are so low.
51. No objections are raised to the current application on highway safety grounds and on this basis I consider it to be acceptable. For the reasons above, I do not consider that this development would impact adversely on highway safety.

Residential Amenity

52. The proposed building is relatively isolated from existing dwellings with the exception of the property on the other side of Olantigh Road. This is set back

from the street and there would be a distance of some 35m between the closest residential property and the west (closest) elevation of the new building. As a consequence of this separation, I do not consider that the proposed building would be overbearing. It should be noted also that the building has been organised on the site so that the lower part of the building (the sports hall) is on the outside elevation. This elevation has no openings which would preclude overlooking of the residential properties and also help reduce noise transmission.

53. I do not consider that the proposed development would be harmful to residential amenity.

Trees

54. The proposed development would require the removal of 1 tree and a small tree group from within the site. These are small specimens and the Tree Officer has raised no objections to their removal. A significant number of new trees are proposed to be planted along the boundary with the parking area and within the space currently occupied by the cabins.
55. An appropriate condition will be attached to secure the provision of these trees once the cabins are removed.

Ecology

56. The survey work identified the presence of slow worm, common lizard and grass snake. No objections are raised subject to a condition.

SUDS

57. Due to the sites layout it is considered likely that sufficient space is available to accommodate appropriately sized soakaways to provide compliance with Ashford Borough Council's SPD. No objections are raised and appropriate condition is attached.

Human Rights Issues

58. I have also taken into account the human rights issues relevant to this application. In my view, the "Assessment" section above and the Recommendation below represent an appropriate balance between the interests and rights of the applicant (to enjoy their land subject only to reasonable and proportionate controls by a public authority) and the interests and rights of those potentially affected by the proposal (to respect for private life and the home and peaceful enjoyment of their properties).

Working with the applicant

59. In accordance with paragraphs 186 and 187 of the NPPF, Ashford Borough Council (ABC) takes a positive and proactive approach to development proposals focused on solutions. ABC works with applicants/agents in a positive and proactive manner as explained in the note to the applicant included in the recommendation below.

Conclusion

60. Whilst this proposal predates a formal masterplan for the WYE3 site, the development is not considered to prejudice the masterplan. The use is acceptable in principle being ancillary development to the Wye school which is on an existing educational site. Furthermore, Wye Neighbourhood Plan policy (WNP11a) provides for the continued use of part of the former college site for education. Therefore refusing the application on grounds relating to prematurity in relation to the masterplan would be unjustified in officer's view. National government policy affords great weight to the need to create, expand or alter schools.
61. I do not consider that the proposed development would have an adverse visual impact on the AONB or impact adversely on the conservation area. Whilst it is a large building, its location within the site between Arboretum; existing development and boundary planting helps to 'contain it'. It would also be seen in the context of the Kempe Centre, a building of comparable size. It is sufficiently far from the conservation area so as not to have a harmful impact. The scale and simplicity of the form is to a large extent dictated by the use and the proposed amendments to the elevations do add interest do help lift the building in my view.
62. I am satisfied that the proposal would not adversely affect highway safety or have an adverse impact on residential amenity. Amendments to the scheme have addressed issues relating to the adequacy of on-site parking and traffic calming and sight lines within Olantigh Road. Lastly, I am assured that an acceptable drainage scheme can be achieved which meets the requirements of the Council's SPD.

Recommendation

Permit

Subject to the following conditions and notes:

1. The development hereby permitted shall be begun before the expiration of 3 years from the date of this decision.

Reason: To comply with the requirements of Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2. Written details including source/manufacturer, and samples of bricks, tiles and cladding materials to be used externally shall be submitted to and approved in writing by the Local Planning Authority before the development is commenced and the development shall be carried out using the approved external materials.

Reason: In the interests of visual amenity.

3. All glazing within the east elevation of the proposed teaching block shall be non-reflective glazing.

Reason: In the interest of visual amenity.

4. Before any works are carried out the following details shall be submitted to and approved in writing by the local planning authority and the works thereafter shall be carried out in accordance with the approved details.

- (a) 1:20 scale sections and elevations of typical elevation details to both the sports hall and classroom block
- (b) 1:20 scale drawings illustrating proposed parapet detailing
- (c) Details of mechanical ventilation or flues to be installed including location, dimensions, colour and material
- (d) Details of rainwater goods

Reason: In the interest of visual amenity

5. Prior to the commencement of the development, a Construction Management Plan shall be submitted to and approved in writing by the Local Planning Authority and local Highway Authority. This shall include details of the following:

- Wheel washing facilities/measures to prevent debris and spoil and the discharge of surface water onto the public highway

- Provision of construction vehicle loading/unloading and turning facilities prior to commencement of work on site and for the duration of construction

- Access point for HGVs and site personnel
- Provision of parking facilities for site personnel and visitors prior to commencement of work on site and for the duration of construction
- Dust suppression methods
- Plant and noise generated from operation of vehicles and machinery
- Fencing/hoardings
- Lighting
- HGV routing
- Hours of operation with hours restricted to between 0730 to 1800 hours (Monday to Friday) and 0730 to 1300 hours (Saturday) with no working activities on Sunday or Bank and Public Holidays.
- Any temporary traffic management/signage required

All details of construction management plan shall be adhered to during the construction period.

No HGVs shall reverse from the site onto the public highway without assistance from a Banksman.

Reason: In the interests of highway safety and the amenities of neighbouring residents.

6. The area shown on the drawing number WP094 Rev A as vehicle parking space and turning shall be provided, surfaced and drained in accordance with details submitted to and approved in writing by the Local Planning Authority before the use of the new building on site is commenced and shall be retained for the use of the occupiers of, and visitors to, the development, and no permanent development, whether or not permitted by the Town and Country Planning (General Permitted Development) Order 2015 (or any Order revoking and re-enacting that Order), shall be carried out on that area of land so shown or in such a position as to preclude vehicular access to this reserved parking space.

Reason: Development without provision of adequate accommodation for the parking of vehicles is likely to lead to parking inconvenient to other road users

7. A Parking Management Plan for the parking area showing how the spaces shall be managed for the differing parking requirements ie. staff; pupil drop off/pick up etc. shall be submitted to and approved in writing by the Local Planning Authority prior to the occupation of the building.

Reason: In the interest of highway safety

8. Prior to first occupation of the new school building secure, covered cycle parking facilities shall be provided in accordance with details previously submitted and approved in writing by the Local Planning Authority. The parking shall be permanently retained.

Reason: To promote sustainable modes of transport

9. The onsite vehicle parking facilities shall be made available for the use of any out of hours weekend functions or sports events.

Reason: In the interest of highway safety

10. Prior to occupation of the new school building, the off-site highways works shown on Plan JNY8974-02 REV C shall be constructed and delivered.

Reason: In the interest of highway safety

11. No surface water shall discharge onto the public highway.

Reason: In the interests of highway safety.

12. Prior to the commencement of development the applicant, or their agents or successors in title, will secure and implement:

- i archaeological field evaluation works in accordance with a specification and written timetable which has been submitted to and approved by the Local Planning Authority; and
- ii further archaeological investigation, recording and reporting determined by the results of the evaluation, in accordance with a specification and timetable which has been submitted to and approved by the Local Planning Authority

Reason: To ensure that features of archaeological interest are properly examined and recorded.

13. No development shall be commenced until:

- a site investigation has been undertaken to determine the nature and extent of any contamination, and
- the results of the investigation, together with an assessment by a suitably qualified or otherwise competent person, and details of a scheme to contain, treat or remove any contamination, as appropriate, have been submitted to and approved by the Local Planning Authority.

Prior to the first use of the development hereby permitted or, where the approved scheme provides for remediation and development to be phased, the occupation of the relevant phase of the development:

- the approved remediation scheme shall be fully implemented (either in relation to the development as a whole or the relevant phase, as appropriate), and
- a Certificate shall be provided to the Local Planning Authority by a suitably qualified or otherwise competent person stating that remediation has been completed and the site is suitable for the permitted end use.

Thereafter, no works shall take place within the site such as to prejudice the effectiveness of the approved scheme of remediation.

Reason: To prevent pollution of the water environment and to avoid risk to the public, buildings and the environment when the site is develop

14. No development shall commence until plans and particulars of a sustainable drainage system (including the details below) for the disposal of the site's surface water has been submitted and approved by Ashford Borough Council.

The final drainage plan for the scheme will be approved by Ashford Borough Council to ensure that surface water runoff from the site is being dealt with appropriately and in line with Ashford Borough Council's Sustainable Drainage SPD.

The submitted system shall comprise retention or storage of the surface water on-site or within the immediate area in a way which is appropriate to the site's location, topography, hydrogeology and hydrology.

Surface water runoff should be dealt with within the application boundary via suitable methods approved by Ashford Borough Council where possible. Proposals should identify any overland flow paths, channelling of flows, or piped flows along with the final point of discharge of the water from the site should be identified.

Infiltration test results must be provided and undertaken in accordance with requirements from BRE Digest 365, with test locations identified.

Soakaways should be sized and designed in accordance with KCC "The Soakaway Design Guide" – July 2000.

The submitted system shall be designed to (i) avoid any increase in flood risk, (ii) avoid any adverse impact on water quality, (iii) return the water to the natural drainage system as near to the source as possible and (viii) operate both during construction of the development and post-completion.

The submitted details shall include identification of the proposed discharge points from the system, a timetable for provision of the system and arrangements for future maintenance (in particular the type and frequency of maintenance and responsibility for maintenance). The approved system shall be maintained in accordance with the approved details and shall be retained in working order until such time as the development ceases to be in use.

No drainage systems for the infiltration of surface water drainage into the ground should occur without the express written consent of Ashford Borough Council. It must be demonstrated that there is no resultant unacceptable risk to controlled waters, or groundwater aquifers.

If, during development, contamination not previously identified is found present then no further development (unless otherwise agreed in writing with the local planning authority) shall be carried out until the developer has submitted a remediation strategy detailing how this unsuspected contamination shall be dealt with. Written approval from Ashford Borough Council must be obtained. The remediation strategy shall be implemented as approved.

Reasons: In order to reduce the impact of the development on flooding, manage run-off flow rates, protect water quality and improve biodiversity and the appearance of the development pursuant to Core Strategy Policy CS20 Sustainable Drainage.

To prevent pollution of controlled waters and comply with the National Planning Policy Framework.

15. No development shall take place (including any ground works, site or vegetation clearance) until a method statement for ecological mitigation has been submitted to and approved in writing by the local planning authority. The content of the method statement shall include the:

- a) Purpose and objectives for the proposed works:
- b) Detailed design(s) and/or working method(s) necessary to achieve stated objectives including updated surveys where necessary;
- c) Extent and location of proposed works, including the identification of a suitable receptor site, shown on appropriate scale maps and plans;
- d) Timetable for implementation, demonstrating that works are aligned with the proposed phasing of construction;
- e) Persons responsible for implementing the works, including times during construction when specialist ecologists need to be present on site to undertake/oversee works;
- f) Use of protective fences, exclusion barriers and warning signs;
- g) Initial aftercare and long-term maintenance (where relevant);

The works shall be carried out in accordance with the approved details and shall be retained in that manner thereafter.

Reason: In the interests of biodiversity

12. The approved development shall be carried out in such a manner as to avoid damage to the existing trees, including their root systems, and other planting to be retained by observing the following:
- (a) All trees to be preserved shall be marked on site and protected during any operation on site by temporary fencing in accordance with BS 5837:2012, (Trees in relation to design, demolition and construction - recommendations) and in accordance with the approved Tree Protection Plan (C122790-02-01_Rev A) and any approved Arboricultural Method Statement, to the satisfaction of the Local Planning Authority. Such tree protection measures shall remain throughout the period of construction
 - (b) No fires shall be lit within the spread of branches or downwind of the trees and other vegetation;
 - (c) No materials or equipment shall be stored within the spread of the branches or Root Protection Area of the trees and other vegetation;

- (d) No roots over 50mm diameter shall be cut, and no buildings, roads or other engineering operations shall be constructed or carried out within the spread of the branches or Root Protection Areas of the trees and other vegetation;
- (e) Ground levels within the spread of the branches or Root Protection Areas (whichever the greater) of the trees and other vegetation shall not be raised or lowered in relation to the existing ground level, except as may be otherwise agreed in writing by the Local Planning Authority.
- (f) No trenches for underground services shall be commenced within the Root Protection Areas of trees which are identified as being retained in the approved plans, or within 5m of hedgerows shown to be retained without the prior written consent of the Local Planning Authority. Such trenching as might be approved shall be carried out to National Joint Utilities Group recommendations.

Reason: Pursuant to Section 197 of the Town and Country Planning Act 1990 and to protect and enhance the appearance and character of the site and locality in accordance with Policy EN1 of the Local Plan.

13. In this condition a “retained tree or shrub” is an existing tree or shrub which is to be retained in accordance with the approved plans and particulars; and paragraphs (a) and (b) below shall have effect until the expiration of 5 years from the date of the (occupation of the building/commencement of use of the approved development) for its permitted use.
- a. No retained tree or shrub shall be cut down, uprooted or destroyed, nor shall any retained tree be pruned in any manner, be it branches, stems or roots, other than in accordance with the approved plans and particulars, without the prior written approval of the LPA. All tree works shall be carried out in accordance with BS3998:2010 *Recommendations for Tree Work*).
 - b. If any retained tree or shrub is cut down, uprooted, destroyed or dies, another tree shall be planted at the same place and that tree shall be of such size and species, and shall be planted at such time, as may be specified in writing by the LPA.

Reason: Pursuant to Section 197 of the Town and Country Planning Act 1990 and to protect and enhance the appearance and character of the site and locality.

16. A landscaping scheme for the site (which may include entirely new native planting, retention of existing planting or a combination of both) shall be submitted to and approved in writing by the Local Planning Authority before the development is commenced. This landscaping scheme shall include the landscaping measures highlighted in the Kent Downs AONB Unit's letter of 8th May 2017. Thereafter, the landscaping/tree planting scheme shall be carried out fully within 12 months of the completion of the development in accordance with the approved details. Any trees or other plants which within a period of five years from the completion of the development die, are removed or become seriously damaged or diseased shall be replaced in the next planting season with others of a similar size and species unless the Local Planning Authority give prior written consent to any variation.

Reason: In order to mitigate for the development and protect and enhance the amenity of the area.

17. The details of soft landscape works required in condition **14** above shall include planting plans; written specifications (including cultivation and other operations associated with plant and grass establishment); schedules of plants, noting species, plant sizes and proposed numbers/densities where appropriate; and an implementation programme.

Reason: To ensure that adequate details of the proposals are submitted in the interests of the protection and enhancement of the area.

16. The development approved shall be made available for inspection, at a reasonable time, by the local Planning authority to ascertain whether a breach of planning control may have occurred on the land (as a result of departure from the plans hereby approved and the specific terms of this permission/consent/approval).

Reason: In the interests of ensuring the proper planning of the locality, the protection of amenity and the environment, securing high quality development through adherence to the terms of planning approvals and to ensure community confidence in the operation of the planning system.

17. The development shall be carried out in accordance with the plans listed in the section of this decision notice headed Plans/Documents Approved by this decision, unless otherwise agreed by the Local Planning Authority.

Reason: To ensure the development is carried out in accordance with the approval and to ensure the quality of development indicated on the approved plans is achieved in practice.

Notes to Applicant

1. Working with the Applicant

In accordance with paragraphs 186 and 187 of the NPPF Ashford Borough Council (ABC) takes a positive and proactive approach to development proposals focused on solutions. ABC works with applicants/agents in a positive and proactive manner by;

- offering a pre-application advice service,
- as appropriate updating applicants/agents of any issues that may arise in the processing of their application
- where possible suggesting solutions to secure a successful outcome,
- informing applicants/agents of any likely recommendation of refusal prior to a decision and,
- by adhering to the requirements of the Development Management Customer Charter.

In this instance:

- the applicant/agent was updated of any issues after the initial site visit,
- was provided with pre-application advice,
- the applicant/agent responded by submitting amended plans, which were found to be acceptable
- The applicant was provided the opportunity to submit amendments to the scheme/address issues.
- The application was dealt with/approved without delay.
- The application was considered by the Planning Committee where the applicant/agent had the opportunity to speak to the committee and promote the application.

2. It is the responsibility of the applicant to ensure, before the development hereby approved is commenced, that all necessary highway approvals and consents where required are obtained and that the limits of the highway boundary are clearly established in order to avoid any enforcement action being taken by the Highway Authority

Background Papers

All papers referred to in this report are currently published on the Ashford Borough Council web site (www.ashford.gov.uk). Those papers relating specifically to this application may be found on the [View applications on line](#) pages under planning application reference 16/01548/AS)

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Annex 1



Ashford Borough Council



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